

Application Number	Date of Appln	Committee Date	Ward
121465/FO/2018	25th Oct 2018	11th Apr 2019	Whalley Range Ward

Proposal Proposed conversion of existing loft space with rear dormer extension and erection of a three storey rear extension to create 3 no. Class C3a additional apartments (11 no. apartments in total) with associated elevational alterations to the existing building, the reconfiguration of external space to form amenity space, car parking, bin storage, cycle storage, landscaping and new boundary treatments

Location 52 Alness Road, Manchester, M16 8HW

Applicant Mr Peter Armistead, Crossland Road,

Agent Mr Simon James, Simon James Arq Ltd, Flat 1, 346 Barlow Moor Road, Manchester, M21 8AY

Site and Area Description

The application site comprises a large Victorian double fronted four storey property including accommodation within the basement. The property currently contains 8 no. one bedroom flats though it retains much of its original character from when it was a single family dwellinghouse. The application states that the property was originally converted to 8 apartments approximately 30 years ago.

The character of the property is a mainly red facing brick building with an original slate roof, with two bays either side to the front, and feature arches over the entrance door and the windows. There are detailed brick work features on the front elevation, a decorative porch and two existing dormer window structures to the front elevation of the roof. The rear elevation of the building is much plainer in detail as would be expected for this period of property and includes stone cill and brick curved header detailing only.

A photograph of the front of the building can be seen below:



The boundary treatment to the front of the property is comprised of the original stone wall together with stone feature gateposts. However, the original profile stone coping stones on the top of the wall are hidden below an overhanging hedge. The northern and southern boundaries are also traditional brick garden walls, however again some of these are hidden from view due to mature landscaping around the site. The shared boundary to the rear of the site is a modern timber panel fence with concrete posts.

The property is then set in a mixture of hard and soft landscaping. This is predominantly hard landscaping comprised of either tarmac or concrete. There is some soft landscaping within the front garden around the edges and an area of amenity space in the rear garden which is comprised of grass and some further shrub planting. There is an existing bicycle shed that is in a poor state of repair and space for 6 cars to be parked, however these are currently not demarcated.

The application outlines that the property is currently in full occupation with the 8 no. 1-bedroom apartments fully let. The exterior is in an acceptable condition but it is stated that the building is reaching a point where a refurbishment would benefit the longevity of the building and stop any deterioration. The interior is very badly laid out, very out of date and does not satisfy current building regulations.

This part of Whalley Range is characterised by large detached and semi-detached Victorian properties, many of which have historically been subdivided into smaller units of accommodation. These properties then sit aside some late 20th century low-rise apartment blocks and further low-rise two storey housing. Many of the roads are characterised by street trees.

The application site is bounded on all side by residential properties with those immediately either side of the site being low-rise two storey semi-detached houses. The property at No. 50 Alness Road is currently occupied as a nursing home.

The application site is not located within a Conservation Area and is not designated as a Listed Building. The nearest Conservation Area to the application site is the Whalley Range CA, which is located a number of streets away to the north.

Development Proposals

Planning permission is sought for the extension and refurbishment of this property to create 3 no. new two bedroom self contained apartments. This development would see the introduction of three further units of accommodation, the 11 resultant units would constitute: 8 no. existing one bedroom flats and 3 no. new two bedroom flats, two of which are duplex flats within the new extension and one within the enlarged attic space.

The development proposals for the extension to the rear of this property have changed significantly since the original submission. The plans submitted initially for the application included a substantial four storey extension to the rear including a roof garden and large flat roof dormer that extended across the full width of the existing roof. The extension projected 10.7 m from the rear of the existing property, was 8.2 m in width and 7.3 m in height. The extension also included the provision of

an undercroft parking area due to the amount of footprint space required for the large extension.

Following detailed negotiations with the applicant, the scheme now includes a much more modest three storey rear extension to the rear, with one of the floors being at the lower ground level. The amended extension would project 6.3 m from the rear elevation (4.4 metre lesser projection than the previous proposal), is 8.5 m in width at its widest part and is 5.4 m in height from the existing ground level (1.9 metre lesser in height than the previous proposal). The size of the extension is significantly smaller than the main building, being almost 3.5m narrower in width and over 2.5m below the existing eave height of the main building.

The proposed extension has a flat roof design with a mix of brickwork to match the existing property and more modern cladding to mark where the old meets the new extension. A rear dormer is also proposed, which is stepped in towards the centre to give the appearance of two smaller dormers to match the design seen on the front of the property.

To the site frontage, 2 car parking spaces would be formalised whilst retaining an existing mature tree. A concealed bin store would be provided, along with some additional soft landscaping. A new pedestrian entrance is to be provided within the existing boundary wall and the existing vehicular access is to be widened. The car parking spaces would be set behind the existing wall and the retained existing hedge and shrubs.

To the rear, 7 formalised car parking spaces would be introduced, with two spaces being served by electric vehicle charging points. A new secure bicycle store is to be provided for 11 bicycles along with an electric bicycle charging point. A new soft landscaped amenity area is to be provided along with newly planted borders and 2 new trees. The newly extended light wells around the lower ground floor of the building will be rationalised and new private garden areas created for the lower ground flats.

It has been confirmed within the Design and Access Statement that all the main exterior materials will be replaced with like for like. The front stone garden wall and side brick garden walls will be repaired, cleaned and repointed. The stone gateposts will be retained and refurbished. A new 500mm timber latt fence will be introduced to the top of the existing boundary walls along the northern and southern boundaries to increase the current heights of 1.2m to 1.4m up to 1.9m in overall height to enhance the security and privacy of the side boundaries. The rear boundary fence which is approximately 1.8m tall will be over clad with timber latt to match the new side boundaries.

Consultations

Local Residents

9 letters of objection were received from 6 different local properties in response to the original application. The comments made can be summarised as follows:

- The height of the proposed extension is a concern, due to overshadowing and blocking sunlight/daylight.
- The roof garden and balconies would result in overlooking and loss of privacy.
- Size - It is far too large for the site and would not be in keeping with the architecture of the local surroundings.
- The noise pollution for major works like this would be lengthy and detrimental. Further noise pollution generated from the additional flats and cars that would be parking in the new renovated car park at the back. This will also lead to an increase in air pollution, something that we want to avoid in the garden areas but also for the climate in general. Residents enjoy their gardens to escape from hustle and bustle of city life and this would be adversely affected should this go ahead.
- The likely increase in cars would cause traffic congestion on Alness Road, which is already very congested due to St Bede's school run.
- From the significant increase in flats, this would cause light pollution into our property and garden, as well as neighbouring ones.
- The location of the application is highly sensitive, being adjacent to a care home for various vulnerable groups who need to be protected. The most direct effect would be on them, which is very disappointing.
- This is not an application based on residential need but is purely based on profit, which must be taken sensibly into account. This appears to be a major project for profit only and does not address any social housing needs
- The local habitat will be damaged by the development. Bats, hedgehogs and Owls all reside in or visit this block, alongside our more familiar foxes, birds, toads, frogs and countless insects and of course bees.
- The balance in our neighbourhood between social housing, rented property and homeowners is long established and gives this neighbourhood a character. A precedent would be set and this may see further development on garden plots that would tip the balance in our area.
- The applicant runs his property business from Canada and there have been ongoing issues with other properties owned by him in relation to poor maintenance.
- Whalley Range is indeed a cosmopolitan location with some parts having been declared a conservation area thanks to their wide, tree-lined streets and large red brick villas. However, we do not require further red bricks villas, or to be precise in this case, red brick monstrosities.

Following the submission of the amended proposals, a full 21 day re-notification of local residents and contributors was completed. Following this, 5 further letters of objection have been received. The comments made on the amended proposals are as follows:

Comments made previously in relation to the following issues still stand:

- Impact on wildlife;
- The balance of social housing, rented properties and homeowners being affected;
- The noise pollution from the major works;
- The overshadowing and loss of privacy caused by the extension;

- The extension being too high and obtrusive;
- Traffic congestion from the increase in cars;
- Light pollution from the new flats;
- Impact on the nursing home at No. 50 Alness Road;
- The precedent of building beyond the current rear building line; and
- The applicant not sufficiently maintaining their properties in this area.

New comments received for the amended proposals include the following:

- Increased demand on the drains, which may not cope with the additional flats; and
- Increased pollution and the impact on local children playing in their gardens

Ward Councillors

Comments were received from Ward Councillors Stogia and Razaq in relation to the original scheme submitted for planning permission. The comments made can be summarised as follows:

- Overdevelopment: the proposed conversion is excessive in terms of the quantity of buildings proposed and the intensity of use for the site. What is proposed is excessive in terms on the impact on local amenity and character of the area. It is noted that the site is just outside the conservation area, but it is felt very strongly that the impact of the character of the local area will be significantly compromised.
- Overlooking: The proposed development will use significant space at the garden of the existing property, the extension is very near other properties and will overlook adjoining properties. We are also very concerned about the roof garden and high level windows which will be overlooking the residential homecare facility located near the property.
- Loss of light to the excising properties. The proposed development will have a huge impact on the nearby properties reducing the amount of light entering the windows to an unreasonable degree.
- Further to the above, Councillors have had a number of representations from residents both via email as well as at our advice surgery and there is very strong feeling in the community that this is not the right proposal for the area. Councillors agree with the residents.

No further comments have been received following the submission of the amended proposals.

Highway Services

The site is considered to be suitably accessible by sustainable modes and is in close proximity to a range of public transport facilities. It is anticipated that the proposals are unlikely to generate a significant increase in the level of vehicular trips therefore they do not raise any network capacity concerns.

From a highway perspective the front boundary proposals with new pedestrian access is acceptable. The vehicle access will be slightly widened and will lead to

parking to the front and rear for 9 cars. Whilst the in curtilage car parking has been reduced to nine spaces (82% provision), the layout is considered to be an improvement on that previously submitted. Given that 100% secure cycle parking is now being provided to the rear, we consider that the amount of overall parking is acceptable.

A bin store area is being provided to the front so that bins can be presented for collection more easily and this is supported.

It is recommended that prior to the commencement of the development a detailed construction management plan outlining working practices during development is submitted to and approved in writing by the local planning authority.

Environmental Health

No objections to the proposals subject to the inclusion of conditions relating to construction management, waste management and contaminated land.

Flood Risk Management Team

A condition is requested that requires the submission of a surface water drainage strategy prior to the commencement of the development.

Arboriculture

The footprint of the proposed new development does not fall within the root protection zones of any of the offsite trees. There are a number of offsite trees which will require root protection during the installation of the new proposed car park area. Any excavation works within the tree protection zones of the offsite trees must be done so in accordance to BS:5837. On this basis, there would have no objection from an Arboricultural perspective as no trees will be impacted by the additional parking spaces.

Greater Manchester Ecology Unit

Due to the creation of accommodation within the attic space, a request was made for the completion of a preliminary bat report to be completed and submitted. However, existing photographs of the attic space was provided by the applicant that showed that most of this space is already currently occupied. Therefore, GMEU have confirmed that on this basis, it is possible to include an informative that requests that if a bat is found, all work should cease immediately and a suitably licensed bat worker employed to assess how best to safeguard the bat(s).

Policy Context

The Development Plan consists of:

- The Manchester Core Strategy (2012); and
- Saved policies of the Unitary Development Plan for the City of Manchester (1995)

The Core Strategy Development Plan Document 2012 -2027 ("the Core Strategy") was adopted by the City Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) and sets out the long term strategic planning policies for Manchester's future development.

A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents as directed by the National Planning Policy Framework (NPPF).

The NPPF requires application to be determined in accordance with the Development Plan unless material considerations indicate otherwise.

Manchester Core Strategy Development Plan Document (July 2012)

The relevant policies within the Core Strategy are as follows:

Policy SP1 'Spatial Principles' states that one of the key spatial principles is the emphasis on the creation of neighbourhoods of choice, providing high quality and diverse housing around district centres which meet local needs, all in a distinct environment.

All development should have regard to the character, issues and strategy for each regeneration area. In addition, new development will be encouraged that maximises the potential of the City's transport infrastructure, in particular promoting walking, cycling and the use of public transport.

The policy goes onto to state that development in all parts of the City should:

- Make a positive contribution to neighbourhoods of choice including;
- Creating well designed places that enhance or create character.
- Making a positive contribution to the health, safety and well being of residents;
- Considering the needs of all members of the community;
- Protect and enhance the built and natural environment.
- Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible;
- Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.

The proposed development is considered to be in accordance with policy SP1 in that a high quality residential development will be provided that contributes towards meeting housing growth in the City and creating a high quality neighbourhood for residents to live in. Consideration has been given to minimising the impact on local residents.

Policy T1 'Sustainable Transport' seeks to deliver a sustainable, high quality, integrated transport system to encourage modal shift away from car travel to public transport, cycling and walking. The proposal is located in an area where there is access to a range of public transport modes, whilst also encouraging other forms of transport such as cycling, and electric vehicle charging points.

Policy EN1 'Design principles and strategic character areas'. The proposed development is considered to be a good quality scheme in terms of its design and appearance that would enhance the regeneration of this area of the City.

Policy H1 'Overall Housing Provision' states that the proportionate distribution of new housing, and the mix within each area, will depend on a number of factors, in particular, the need to diversify housing stock in mono tenure areas by increasing the availability of family housing. Policy H1 prioritises residential development on previously developed land, in particular through the re-use of vacant housing or other existing buildings. The redevelopment of this brownfield site would accord with policy H1. Consideration has been given to the design, siting and scale of the extension and the new apartments. In addition, the proposal will also provide accommodation which will be attractive to a diverse range of housing needs through varying accommodation size. The accommodation is generous in size with the new flats being 2 bedroom accommodation.

Policy DM1 'Development Management' all development should have regard the following specific issues:-

- Appropriate siting, layout, scale, form, massing, materials and detail;
- Impact on the surrounding areas in terms of the design, scale and appearance of the proposed development. Development should have regard to the character of the surrounding area;
- Effects on amenity, including privacy, light, noise, vibration, air quality, odours, litter, vermin, birds, road safety and traffic generation. This could also include proposals which would be sensitive to existing environmental conditions, such as noise;
- Community safety and crime prevention;
- Design for health;
- Adequacy of internal accommodation and external amenity space;
- Refuse storage and collection;
- Vehicular access and car parking;
- Effect on biodiversity, archaeological or built heritage;
- Green infrastructure;
- Flood risk and drainage.

The applicant has given careful consideration to the design, scale and layout of the development along with providing solutions address amenity space for the occupants, refuse storage, car parking and cycle parking. Impacts on residential amenity are assessed in the section below.

For the reasons given above, it is considered that the proposal is consistent with the policies contained within the Core Strategy.

Unitary Development Plan (1995)

The majority of the previous UDP policies have been replaced by the Core Strategy, however there are still saved policies that remain extant. The relevant policies for this application are set out below.

Policy DC5 sets down the criteria to be considered in determining applications for flat conversions. The policy establishes the principles for converting large properties and the need for adequate parking, and amenity space.

The application site is already given over to flats. The scheme would see the introduction of three further units, it would improve the standard of accommodation, improve the appearance of the site whilst providing shrubbery and retaining a tree to the frontage, adequately addressing car parking, amenity space, bin storage and the impact on surrounding residential properties. The scheme is therefore considered to accord with saved policy DC5.

National Planning Policy Framework (2018)

The revised NPPF was adopted in July 2018. The document states that the ‘purpose of the planning system is to contribute to the achievement of sustainable development. The document clarifies that the ‘objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs’ (paragraph 7).

In order to achieve sustainable development, the NPPF states that the planning system has three overarching objectives – economic, social and environmental (paragraph 8).

Section 5 ‘Delivering a sufficient supply of new homes’ states that in order to support the Government’s objective of significantly boosting the supply of homes, ‘it is important that a sufficient amount and variety of land can come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay’ (paragraph 59).

Section 8 ‘Promoting Healthy and Safe Communities’ states that planning policies and decisions should aim to achieve healthy, inclusive and safe places (paragraph 91).

Section 9 ‘Promoting Sustainable Transport’ states that ‘significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health’ (paragraph 103).

Section 11 ‘Making effective use of land’ states that ‘planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions’ (paragraph 117).

Section 12 'Achieving Well Designed Places' states that 'the creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this' (paragraph 124).

Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a "presumption in favour of sustainable development". This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out-of-date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Planning Practice Guidance (PPG)

The relevant sections of the PPG are as follows:

Noise states that Local planning authorities' should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.
- Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:
 - engineering: reducing the noise generated at source and/or containing the noise generated;
 - layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
 - using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
 - mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other
- form – the shape of buildings
- scale – the size of buildings
- detailing – the important smaller elements of building and spaces
- materials – what a building is made from

Health and well being states opportunities for healthy lifestyles have been considered (e.g. planning for an environment that supports people of all ages in making healthy choices, helps to promote active travel and physical activity, and promotes access to healthier food, high quality open spaces and opportunities for play, sport and recreation);

Travel Plans, Transport Assessments in decision taking states that applications can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;
- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads.

Other material policy considerations

The Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (Adopted 2007)

This document provides guidance to help develop and enhance Manchester. In particular, the SPD seeks appropriate design, quality of public realm, facilities for disabled people (in accordance with Design for Access 2), pedestrians and cyclists. It also promotes a safer environment through Secured by Design principles, appropriate waste management measures and environmental sustainability.

Manchester Residential Quality Guidance (2016)

The City Council's Executive has recently endorsed the Manchester Residential Quality Guidance. As such, the document is now a material planning consideration in the determination of planning applications and weight should be given to this document in decision making.

The purpose of the document is to outline the consideration, qualities and opportunities that will help to deliver high quality residential development as part of successful and sustainable neighbourhoods across Manchester. Above all the guidance seeks to ensure that Manchester can become a City of high quality residential neighbourhood and a place for everyone to live.

The document outlines nine components that combine to deliver high quality residential development, and through safe, inviting neighbourhoods where people want to live. These nine components are as follows:

- Make it Manchester;
- Make it bring people together;
- Make it animate street and spaces;

Make it easy to get around;
Make it work with the landscape;
Make it practical;
Make it future proof;
Make it a home; and
Make it happen.

Other legislative requirements

Section 149 Equality Act 2010 provides that in the exercise of all its functions the Council must have regard to the need to eliminate discrimination, advance equality of opportunity and foster good relations between person who share a relevant protected characteristic and those who do not. This includes taking steps to minimise disadvantages suffered by persons sharing a protect characteristic and to encourage that group to participate in public life. Disability is a protected characteristic.

Section 17 Crime and Disorder Act 1998 provides that in the exercise of its planning functions the Council shall have regard to the need to do all that it reasonably can to prevent crime and disorder.

Issues

Principle

The application site is located within the South Manchester, as allocated within the Manchester Core Strategy (2012). Policy H6 states that this area will accommodate around 5% of new residential development over the lifetime of the Core Strategy. Outside the district centres priorities will be for housing which meets identified shortfalls, including family housing and provision that meets the needs of elderly people, with schemes adding to the stock of affordable housing. Policy H1 also seeks to ensure good quality housing. The application site is an important site within the area and it is recognised that the building is in need of refurbishment and the existing accommodation within the property needs upgrading. Whilst it is considered that the principle of the development is consistent with planning policy framework, there are detailed matters that require particular attention. This report will therefore consider the following material considerations and determine whether any undue harm will arise as a consequence of the development.

Design

Policy EN1 of the Core Strategy states that opportunities for good design, that enhance the overall image of the City, should be fully realised. This is reiterated within the Guide to Development in Manchester SPD along with the NPPF. It is considered that the design of the proposed development has been to adopt many of the key characteristics of the surrounding area.

The extension would have a more contemporary design than the existing building, but will be complimentary through the use of matching brickwork. Modern materials including a grey fibre cement board and grey powder coated aluminium windows along with large modern proportioned windows will enhance the appearance of the

extension clearly distinguishing the original Victorian property from the newly created later addition. It is considered that this approach to design is acceptable in this context. The property is not located within a Conservation Area and the property is not listed, and therefore, this contemporary design is considered to be appropriate.

The proportions of the extension have altered from the original scheme, now being entirely subordinate to the host building and relating to the existing features to the rear elevation. Through a set back being provided in the middle of the new dormer roof extension on the rear, this has also been designed to be in keeping with the character of the original property but with a contemporary approach. This design give the appearance of two smaller extensions to mirror the existing dormer structures on the front of the property.

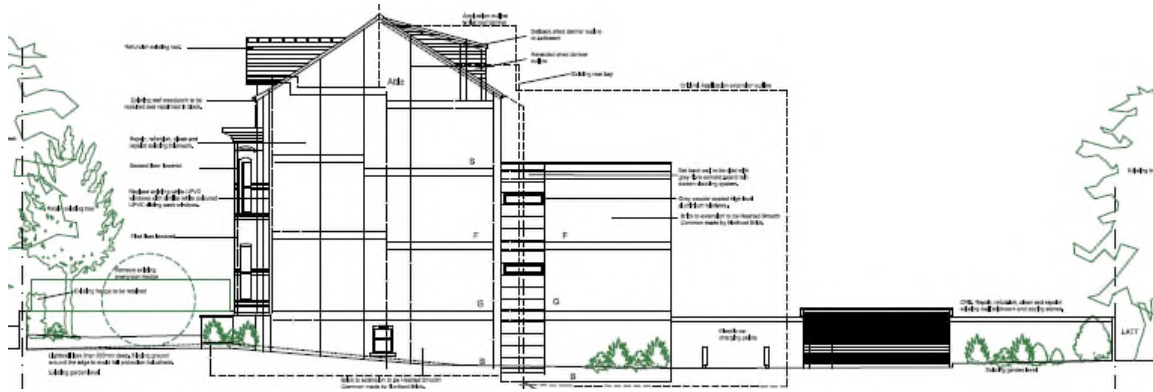
The existing and proposed elevations of the building and the extension can be seen below:



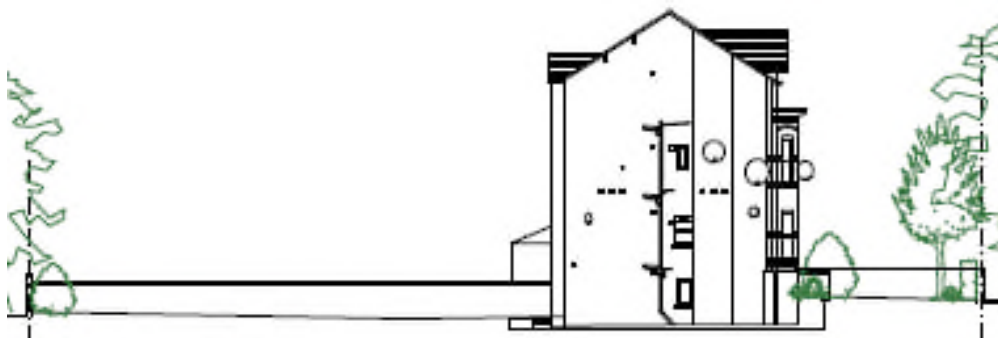
Existing Front Elevation



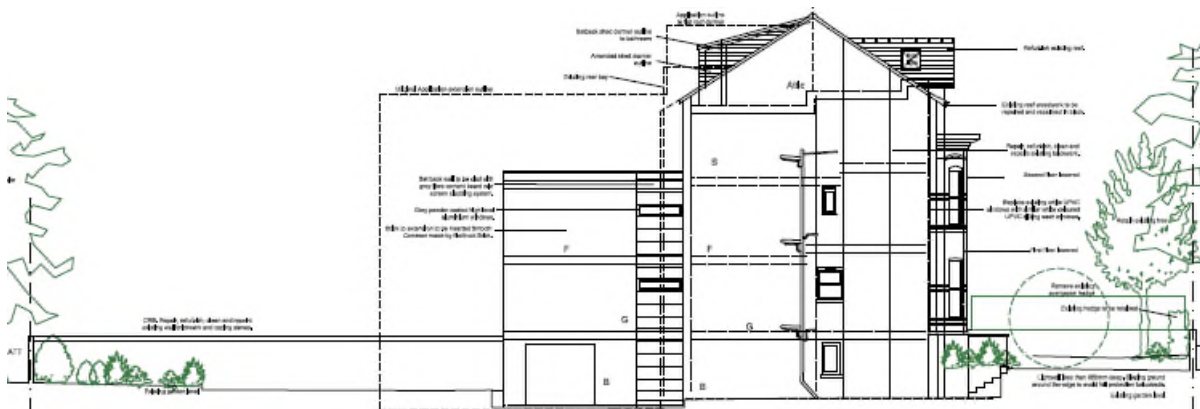
Proposed front elevation



Proposed North Elevation



Existing South Elevation



Proposed South Elevation

It should be noted that the dashed lines shown on the side elevations above are to highlight the outline of the original proposals submitted with the application and the significant decrease in scale of the proposals now being applied for.

It is considered that the appearance of the development will be a good quality development both to streetscene and within the area. The simple arrangements of the elevations along with the quality and use of materials, position of the building on

the site and its scale, will enhance the setting of the existing Victorian property and improve this site. It is recommended that a condition of the planning approval is that the final materials are agreed with the applicant to ensure they are suitable.

Landscaping / Car Parking / Amenity Space

The proposed development includes the provision of both landscaped front and rear gardens, including an ample area of private outdoor amenity space for the future occupants of this development. The planting of new trees is proposed to improve the amount of soft landscaping at the site and the necessary tree protection of the trees in neighbouring gardens is ensured through a condition.

The existing frontage is currently mainly given over to hardstanding. The bins that are currently stored in a prominent location would be sensitively screened from view in a newly created bin store. The frontage would be enhanced by proposed landscaping comprising ground cover plants around the 2 new demarcated car parking spaces which is well screened by the existing brick wall and mature hedge along the frontage.

The unattractive rear garden area that is mainly given over to hard surfacing and a poorly maintained grass and shrub area, would be replaced with formalised car parking for 7 cars, a new secure bicycle storage shed for 11 bikes, and a newly created rear amenity area, which is a mix of grass and patio and is boosted by new planting and separated from the car parking by a new beech hedge. Two new trees are proposed to be planted in the rear garden to improve this space for future occupants. Further private amenity areas are proposed within the expanded light well areas for the lower ground floor apartments.

Looking at the historic pattern of development in the area, ideally there would be a greater amount of soft landscaping provided. However, regard has been paid to the current condition of the rear garden associated with the existing 8 residential units, which has not constituted a useful amenity space to the current occupants of the building nor contributed positively to the residential amenity of any neighbour overlooking the site or the character of the area.

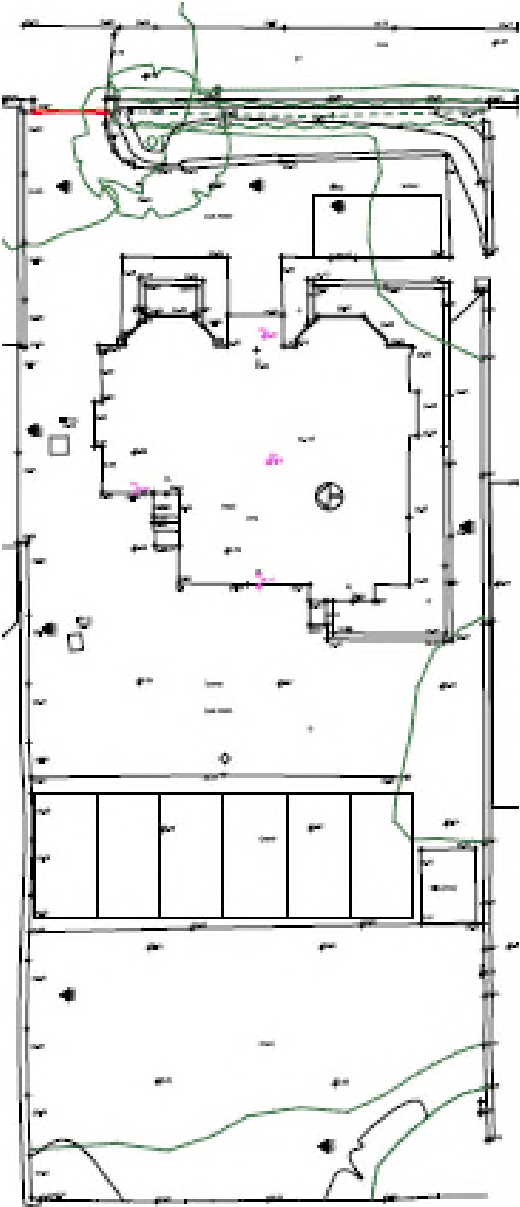
The development proposal constitutes an improvement in this regard. The applicant has also increased the amount of amenity space from the originally submitted drawings, which only indicated a very small area of amenity space. It is now considered that there would be sufficient amenity space provided for the enjoyment of the occupiers of the building.

Policy T1 and T2 of the Core Strategy seeks to encourage modal shifts away from the car and locate new development that is accessible by walking, cycling and public transport. Policy DM1 goes on to state that traffic generation and road safety must be considered as part of new developments.

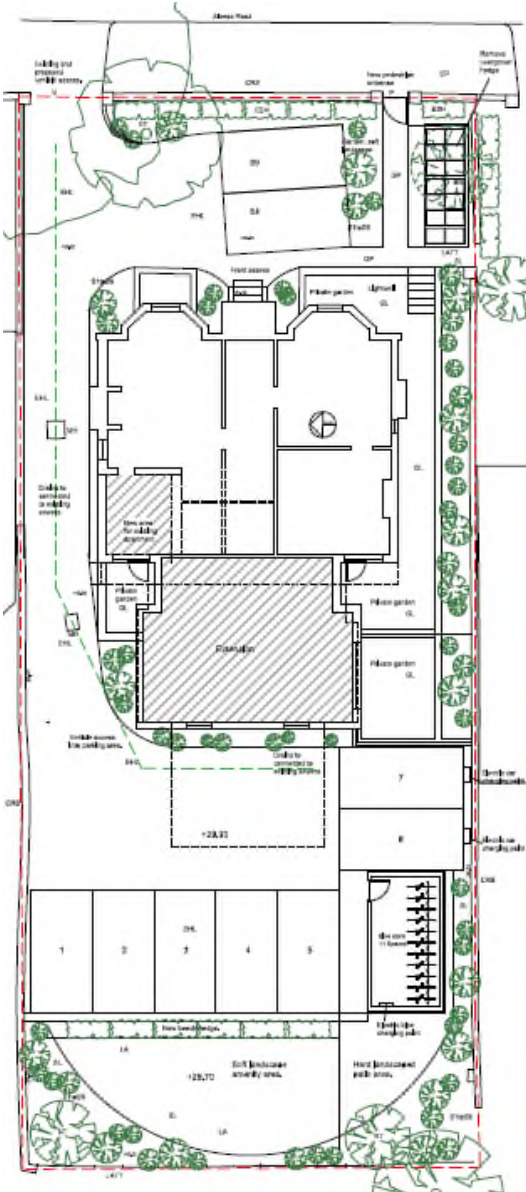
The provision of 9 parking spaces for a development of 11 flats has been assessed by Highway Services. The streets in the immediate area are not heavily parked due to the majority of properties having sufficient off street parking. Despite the 9 spaces constituting a 82% provision, the layout of the site is considered to be an

improvement by Highway Services over the original layout and given that 100% cycle parking provision is being provided along with 2 electric vehicle charging points and one electric bicycle charging point being provided, it is considered that the overall provision of car parking is acceptable for this development. Highway Services have also raised no issues with the access arrangements.

The existing and proposed site layout plans can be seen for comparison below, including the site layout plan for the previously proposed scheme:



Existing Site Layout



Proposed Site Layout

As the proposals only include the provision of 3 additional flats, the comings and goings should be limited and will not have any effect on the capacities of the surrounding roads. Overall, it is considered that the development will have a minimal impact on the local highway network transport and there will be adequate car and cycle provision to serve the needs of the development. The proposal therefore accords with policies SP1, T1 and DM1 of the Core Strategy.

Proposed Accommodation

As outlined within the Design and Access Statement, the proposed development is, in addition to the extension, to significantly enhance the existing accommodation that is in need of updating. This includes a number of features like the interiors, uninsulated roof, wall and floors, windows and the brickwork needs to be cleaned, repaired and re-pointed. The scheme will also provide better boundary treatment, improvements to access landscaping, better defined car parking, soft landscaping, secure bike storage and better located semi enclosed bin store areas.

The rear elevation windows will be reconfigured to enhance the quality of light received within the existing accommodation and together with the main apartment living spaces, will be oriented to the westerly sunlight and views over the garden. This is in response to the Residential Quality Guidance recommendations that designs should address the need for increased levels of natural light to reduce the need for artificial light.

The proposal is to convert the existing 8 one bedroom flats within the existing building into 8 no. larger one bedroom apartments and 1 no. 2 bedroom flat within the roof space. The extension would then provide 2 no. additional duplex 2 bedroom flats to make a total of 11 self-contained apartments over 5 floors. The existing flats will be retained as existing, refurbished and where possible more efficiently laid out around a better located circulation core accessed through the existing main entrances. The existing northern apartments will be improved using the additional floor space brought into use by infilling the north-western corner of the building. The 2 new 2-bedroomed apartments will have a duplex configuration offering the greatest open space, views and light to the living area. The 3 no. new apartments being provided are measured at 75 sqm, 76 sqm and 76 sqm all of which meet the internal space standards outlined within the Residential Quality Guidance. The basement apartments would retain their lightwells and full height private garden amenity spaces would be provided.

Therefore, the layout of the existing building and extension has been designed to provide an appropriate level of privacy for existing neighbouring residents as well as those that will occupy the building. All new apartments are appropriately sized in line with the space standards referenced in the Manchester Residential Quality Guidance. Therefore, the proposed accommodation is considered to be acceptable in this case.

Residential Amenity

Objections from local residents that live around the application site have been received. In relation to an impact on amenity, concerns relate to the scale of the proposed extension and the intensification of the use, the overshadowing and loss of privacy caused by the extension, the provision of the car parking to the rear and the current condition of the site.

It was acknowledged that the scheme as originally submitted was an overdevelopment of this site, and that the substantial extension would have resulted in an unacceptable detrimental impact on the amenity currently enjoyed by the

surrounding properties. However, detailed negotiations have taken place with the applicant, and the height, scale and massing of the proposed extension have been significantly reduced to ensure that the extension proposed would not have an adverse impact upon the surrounding properties. As outlined above, the amended proposals include a 4.4 metre reduction in the projection from the rear of the building and a 1.9 metre reduction in height than the previous proposal. The significant rear dormer extension to the roof has also been reduced in size to address the concerns raised in relation to an overshadowing impact on the adjacent properties.

The extension now proposed is considered to be subservient and of an appropriate scale in comparison to the existing application site and to the properties either side of the site. The roof garden has been omitted from the scheme and the only windows to the sides of the extension proposed would be high level windows, which means that there would be no overlooking created.

The extension proposed would be 3 m from the side boundaries with the adjacent properties on Alness Road, 11.8 m from the rear boundary of the site with 45 Manley Road and a much greater distance from other properties that have objected to the application. Whilst the extension would be due south of the property at 50 Alness Road, the separation distance provided would negate any unduly unacceptable impact with regards to loss of light / overbearing impact. Therefore, it is not considered that there will be a detrimental impact caused to the residential amenity of these properties from either overshadowing or overlooking.

Concerns have also been raised in relation to light pollution from the additional flats. It is not expected that the light emitted from the site will increase significantly through the erection of the extension, however a condition has been included that states that if any lighting at the development causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, then this must be addressed by the applicant.

The introduction of 3 no. additional flats (provision of 6 further bedrooms across all units) would not be considered to constitute overdevelopment of this site that would adversely impact upon residential amenity. As the proposals only include the increase of 3 flats, the additional comings and goings should be limited and should not have a significant effect on the residential amenity currently enjoyed by the surrounding occupants.

The use of the rear area of the site for car parking is an existing arrangement and it was noted at the site visit that only one car was parked at the property. The proposals include the creation of only one additional space to the rear, and the existing boundary treatment and soft landscaping around the site is to be improved to help to screen the formalised parking area. Therefore, it is not considered that the proposals would cause a loss of amenity to surrounding properties. The screening would help to block out light and sound created within this area.

Concerns have also been raised about the potential impact of the development on the vulnerable occupants of the nursing home at 50 Alness Road. As raised above, the proposals only include the increase of 3 apartments and the additional comings and goings from the site will be limited. The proposed extension has been

redesigned to reduce the previously considered significant unacceptable impact on the property at 50 Alness Road and therefore, it is not considered that the development will have an unacceptable impact on these vulnerable occupants.

Therefore, it is considered that this site will benefit from the well designed good quality extension and elevational and site improvement works, and that the development will not create a significant adverse and harmful impact on the residential amenity or outlook experienced from existing residential properties. Therefore, it is considered that the proposals are in accordance with Policy DM1 of the Core Strategy.

Construction Management

Objections have been received from local residents about the potential disturbance to their amenity created by the construction works necessary to build the extension and to upgrade the existing building. It is acknowledged that there would be some impact from the resulting construction works, however this would only be in the short term and the relevant restrictions on construction sites in the City will also apply here in relation to permitted hours of work and considerate neighbour rules. Due to the location of the development adjacent to existing residential properties, a condition has been included to require the submission of a construction management strategy which addresses issues such as how the construction process will be managed at the site, particularly in respect of the operating hours, dust suppression, wheel washing, routing strategies, security and waste.

Other Resident Objections

In addition to the objections received in relation to an impact on residential amenity and highway related matters that are addressed in the sections above, comments have also been received in relation to ecological concerns, and the ongoing maintenance of the property. The Greater Manchester Ecology Unit has been consulted on this application and no concerns have been raised by them in relation to potential harm on the ecological value of the site. An informative has been included in relation to if any bats are found in the attic, and to remind the applicant about the legal requirements within the Wildlife and Countryside Act. A good amount of soft landscaping is being provided at the site along with the retention of some of the existing planting around the site. Therefore, it is not considered that the proposed development would have a detrimental impact on the local habitats or wildlife at the site or within the wider area.

In relation to problems experienced locally with other properties owned by the applicant in terms of ongoing maintenance, even though this is not an issue that can be given any significant weight in the consideration of this application, the concerns are acknowledged and the applicant will be notified of the concerns raised. Through the submission of the application, it appears that the applicant does acknowledge that upgrade works are necessary to the existing property and that efforts are being made through the development proposals to improve the overall appearance of the site for the existing occupants and to the benefit of the wider area.

Waste

A concealed bin store would be provided to the front of the building close to the shared boundary with No. 54 Alness Road. Plans have been submitted showing the final design of the bin store to be constructed, however it is considered that further negotiations are required to ensure the best quality appearance for this structure. Therefore, this is requested through an appropriately worded condition.

The applicant has identified the number of bins that are required having regards to Manchester City Council segregated waste standards and indicated how these will be accommodated in the bin store. It is stated that each kitchen will have separated recycling bin cubes under the kitchen sink in a deep bin drawer. The bins will be moved to the collection point and returned to the bin store, on bin day by the occupants. The submitted waste management strategy has been assessed by Environmental Health and it has been confirmed that the arrangements are acceptable for this development.

Ground conditions

Policy EN18 of the Core Strategy requires that consideration should be given to potential sources of ground contamination and the effect on new developments. It is recommended that a condition of the planning approval is that details should be submitted in relation to the contamination of the ground. Once the remediation strategy has been approved this shall be implemented and a verification report submitted on completion of the development to verify that all the agreed remediation has been carried out. This approach should form a condition of the planning approval in order to comply with policy EN18 of the Core Strategy.

Permitted development

The National Planning Policy Guidance states that only in exceptional circumstances should conditions be imposed which restrict permitted development rights otherwise such conditions are deemed to be unreasonable. It is recommended that a condition of the approval should clearly define the approved residential units under the C3(a) use and to remove the permitted development rights that would normally allow the change of use of a property to a House of Multiple Occupation (HMO) falling within use classes C3(b) and C3(c) without the requirement for formal planning permission. This is to protect this development and its future residents from the problems associated with the change of use of properties to HMO's and to promote family accommodation and sustainability within this neighbourhood.

Conclusion

Careful consideration has been given to the scheme with regards to the siting, scale and appearance of the development proposed. On balance, it is believed that the scheme would provide a good quality development which would improve the appearance of the building and the site. It is not considered that the scheme would have a detrimental impact upon surrounding residential accommodation, subject to appropriate conditions being put in place.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Head of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the approval of the application is proportionate to the wider benefits of approval and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation APPROVE

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Officers have communicated their concerns about the original proposals submitted to the applicant during the course of the planning application and these concerns have been addressed through negotiations and the submission of amended drawings. Appropriate conditions have also been attached to ensure that the development is acceptable. Therefore, the proposal is now considered to be acceptable, in compliance with local and national planning policies and has been determined in a timely manner.

Reason for recommendation

Conditions to be attached to the decision

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Site Location Plan 127 PL OSL1
stamped as received by the Local Planning Authority on the 5th October 2018
Pre-Development Tree Condition Survey

stamped as received by the Local Planning Authority on the 25th October 2018

Drawings:

127 PL EL1 B

127 PL EL2 B

127 PL GA2 B

127 PL GA3 B

127 PL GA4 B

127 PL GA5 B

127 PL GA6 B

127 PL GA7 B

Amended Design and Access Statement

stamped as received by the Local Planning Authority on the 27th February 2019

Waste Management Strategy

Drawing 127-PL-GA1revD

stamped as received by the Local Planning Authority on the 28th March 2019

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

3) In accordance with the principles outlined with the Design and Access Statement, prior to the erection of the above ground structure, final samples and specifications of all materials to be used on the external elevations of the development hereby permitted shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved materials shall then be used in the construction of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Manchester Core Strategy.

4) a) The development hereby approved shall be completed in accordance with the hard and soft landscaping scheme shown on the Proposed Site Layout drawing numbered 127-PL-GA1revD stamped as received by the Local Planning Authority on the 28th March 2019.

b) The approved scheme shall be implemented within a timescale previously agreed in writing by the local planning authority.

c) If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies EN9, EN15 and DM1 of the Core Strategy.

5) a) Before the development hereby approved commences, a report (the Preliminary Risk Assessment) to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas relevant to the site shall be submitted to and approved in writing by the City Council as local planning authority. The Preliminary Risk Assessment shall conform to City Council's current guidance document (Planning Guidance in Relation to Ground Contamination).

In the event of the Preliminary Risk Assessment identifying risks which in the written opinion of the Local Planning Authority require further investigation, the development shall not commence until a scheme for the investigation of the site and the identification of remediation measures (the Site Investigation Proposal) has been submitted to and approved in writing by the City Council as local planning authority.

The measures for investigating the site identified in the Site Investigation Proposal shall be carried out, before the development commences and a report prepared outlining what measures, if any, are required to remediate the land (the Site Investigation Report and/or Remediation Strategy) which shall be submitted to and approved in writing by the City Council as local planning authority.

b) When the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Core Strategy.

6) a) Prior to the commencement of the development, a scheme for the drainage of surface water from the new development shall be submitted for approval in writing by the City Council as the Local Planning Authority. The development shall then be constructed in accordance with the approved details, within a previously agreed timescale.

b) Prior to the first occupation of the extension hereby approved, a verification report shall be submitted, including relevant photographic evidence, that the scheme has been implemented in accordance with the previously approved details.

Reason - The application site is located within a critical drainage area and in line with the requirements in relation to sustainable urban drainage systems, further consideration should be given to the control of surface water at the site in order to minimise localised flood risk pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

7) In this condition "retained tree" means an existing tree, shrub or hedge within or bordering the application site which is to be as shown as retained on the approved plans and particulars; and paragraphs (a) and (b) below shall have effect until the expiration of 5 years from the date of the occupation of the building for its permitted use.

(a) No retained tree shall be cut down, uprooted or destroyed, nor shall any retained tree be topped or lopped other than in accordance with the approved plans and particulars, without the written approval of the local planning authority. Any topping or lopping approved shall be carried out in accordance with British Standard 5387 (Trees in relation to construction)

(b) If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the local planning authority.

(c) The erection of fencing for the protection of any retained tree shall be undertaken in accordance with the approved plans and particulars before any equipment, machinery or materials are brought on to the site for the purposes of the development, and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the local planning authority.

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

8) All tree work should be carried out by a competent contractor in accordance with British Standard BS 3998 "Recommendations for Tree Work".

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

9) Prior to the commencement of the development hereby approved, a detailed construction management plan outlining working practices during development shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include;

- Display of an emergency contact number;
- Details of Wheel Washing;
- Compound locations where relevant;
- Location, removal and recycling of waste;

- Routing strategy and swept path analysis;
- Parking of construction vehicles and staff; and
- Sheeting over of construction vehicles.

Development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents and highway safety, pursuant to policies SP1, EN19 and DM1 of the Manchester Core Strategy (July 2012).

10) The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the proposed extension hereby approved being occupied. The car park shall then be available at all times whilst the site is occupied.

Reason - To ensure that there is adequate car parking for the development proposed when the building is occupied, pursuant to Policies T2, SP1 and DM1 of the Core Strategy.

11) The provision of space and facilities for 11 no. bicycle parking spaces within the development shall be completed in accordance with the drawings numbered 127-PL-GA1revD dated 28/03/19 received by the Local Planning Authority on the 28th March 2019. The approved space and facilities shall then be retained and permanently reserved for bicycle parking.

Reason - To ensure that adequate provision is made for bicycle parking so that persons occupying or visiting the development have a range of options in relation to mode of transport in order to comply with Policies SP1, T1, T2, EN6 and DM1 of the Core Strategy and the guidance provided within the National Planning Policy Framework and the Guide to Development in Manchester Supplementary Planning Document and Planning Guidance.

12) The provision of space and facilities for 2 no. electric vehicle charging points and 1 no. electric cycle charging point within the development shall be completed in accordance with the drawings numbered 127-PL-GA1revD dated 28/03/19 received by the Local Planning Authority on the 28th March 2019. The approved spaces shall be provided at the site prior to the first occupation of the extension hereby approved and these facilities shall then be retained and useable for as long as the development is occupied.

Reason - To ensure that adequate provision is made for electric vehicle and cycle charging so that persons occupying or visiting the development have a range of options in relation to mode of transport and to reduce air pollution within the City in order to comply with Policies SP1, T1, T2, EN6 and DM1 of the Core Strategy and the guidance provided within the National Planning Policy Framework and the Guide to Development in Manchester Supplementary Planning Document and Planning Guidance.

13) The development hereby approved shall be completed and occupied in accordance with Waste Management Strategy stamped as received by the Local

Planning Authority on the 28th March and the layout plan numbered 127-PL-GA1revD dated 28/03/19 received by the Local Planning Authority on the 28th March 2019. The details of the approved scheme shall be implemented as part of the development and shall remain in situ whilst the use or development is in operation.

Reason - In the interests of amenity and public health, pursuant to policies SP1 and DM1 of the Core Strategy and guidance provided within the Guide to Development in Manchester Supplementary Planning Document and Planning Guidance.

14) Prior to the construction of the above ground structure, elevational drawings and specifications for the proposed refuse store and the cycle shelter shown on the proposed site plan numbered 127-PL-GA1revD dated 28/03/19 received by the Local Planning Authority on the 28th March 2019 shall be submitted to an approved in writing by the Local Planning Authority. The development shall then be constructed in accordance with approved drawings prior to the first occupation of the extension hereby approved.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

15) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 14 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Core Strategy.

16) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 as amended by The Town and Country Planning (General Permitted Development) (Amendment) (England) Order 2010 (or any order revoking and re-enacting that Order with or without modification) no part of the development shall be used for any other purpose (including any other purpose in Class C3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 as amended by The Town and Country Planning (Use Classes) (Amendment) (England) Order 2010, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification) other than the purpose(s) of C3(a). For the avoidance of doubt, this does not preclude two unrelated people sharing a property.

Reason - In the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1

and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 121465/FO/2018 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

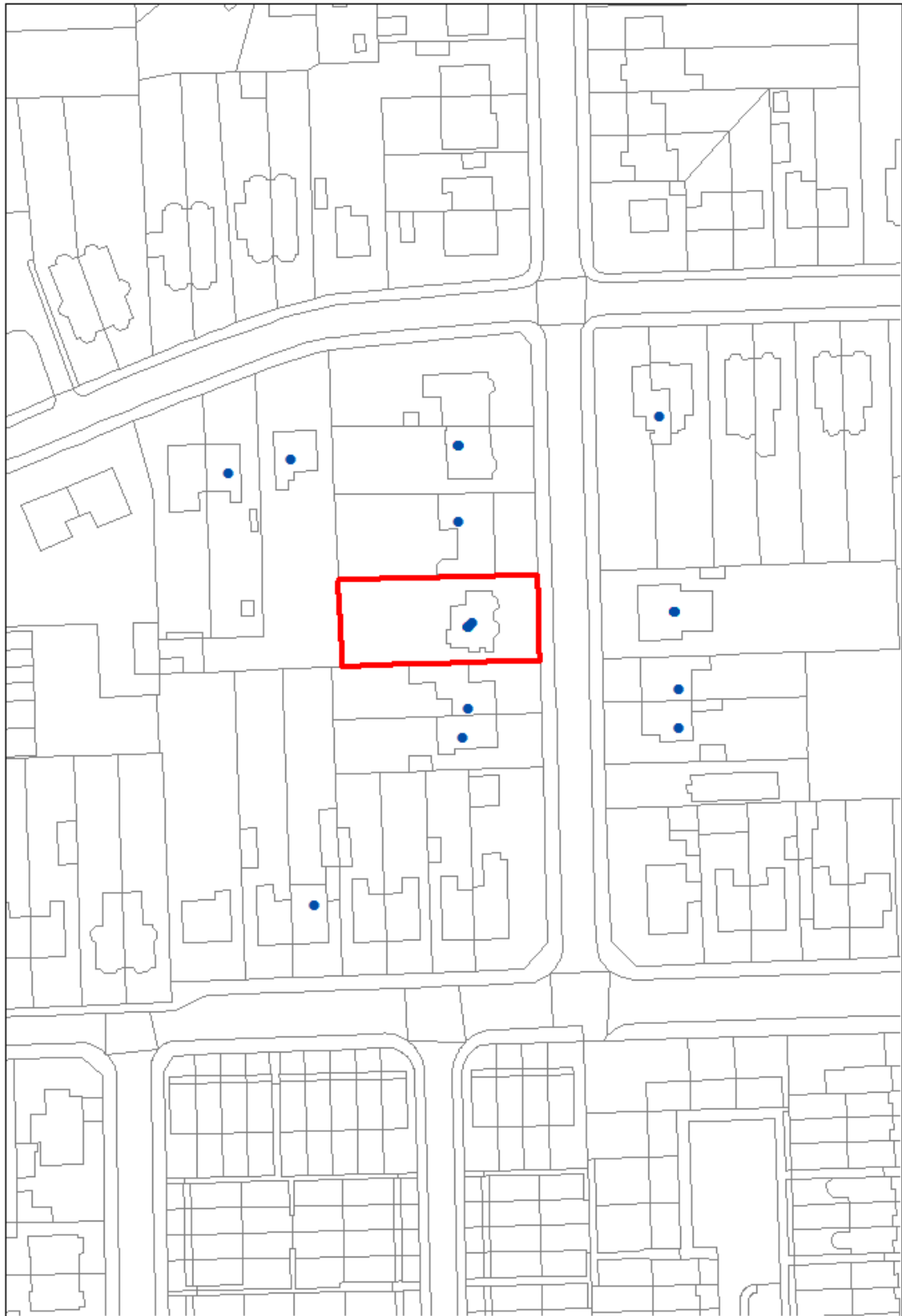
The following residents, businesses and other third parties in the area were consulted/notified on the application:

Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
MCC Flood Risk Management
Greater Manchester Police
Greater Manchester Ecology Unit

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Relevant Contact Officer :	Jeni Regan
Telephone number :	0161 234 4164
Email :	j.regan@manchester.gov.uk



 Application site boundary  Neighbour notification
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